

NC ACCESSIBILITY CODE

2012 NC Bldg Code Ch. 11

2009 ICC/ANSI A117.1

Mandatory: 1 Jun 2012

1/1/19: 2018 NCBC Effective
2009 ICC/ANSI A117.1

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2010 ADA STANDARDS:

- Mandatory 3/15/12
- Copy available: www.ada.gov

Individual Highlights

CBU Q&A

2



CBUs: NCDOT + USPS + NCDOI

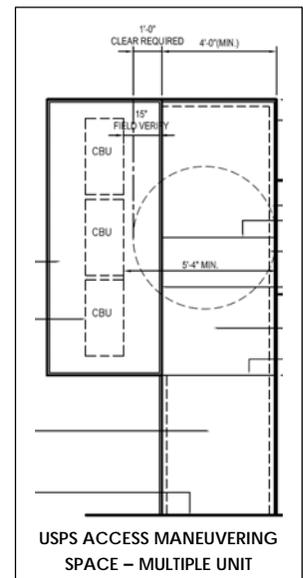
Since the last time Mailboxes + CBUs were addressed [[V216](#); [V711](#); [V712](#); [V714](#); [V715](#)], additional information was received from both the **North Carolina Department of Transportation (NCDOT)** and the **United States Postal Service (USPS)** regarding installation of CBUs. The requirements are applicable to exterior CBUs that are covered or uncovered, and to CBUs in both single family and multifamily developments. [The requirements are attached at the end of this newsletter.]

Summary:

1. **NCDOT Policy for Placement of Mail Cluster Box Units [9/1/15]**
 - Addresses placement of CBUs 'outside of the road right of way to provide safe and convenient parking and pedestrian access for postal patrons and to minimize impedance to roadway operation.'
2. **USPS Cluster Box Units (CBU) Concrete Pad Installation - Interim Pad Policy [2/9/17]** - see last page of attachment.
 - Requires 60" turning circle in front of entire CBU length
 - Min. 1% - max. 2% cross slope away from CBU
 - 48" minimum sidewalk
 - 12" clear from CBU to sidewalk
 - Min. 5'-4" from CBU to furthest edge of sidewalk
3. **NCDOT Parking Requirement + Accessible Route**
 - **ANSI 503** Passenger Loading Zone with curb cut -OR-
 - **NCBC 1106.5** Van space with **ANSI 502.4** access aisle + **NCBC 1106.1** Regular parking space with curb cut.

NOTE: CBU parking is independent of all other parking provisions.

- Reach ranges on exterior single-family CBUs not enforced.
- Alterations of CBUs shall comply with current requirements.



Glazing Bands + Glazing Design Resources

Let's think about good design for a moment and how much we love all those glossy, window-filled structures even though they often create a problem for birds mistaking the glazing's reflection for the clear open sky. [NYC Apple, below.]

- When you design /install glazing to provide that 'open (office) feeling,' have you considered the effect on the visually impaired?
- When you design glass panels/entrance doors adjacent to paneled glazing, how readily apparent is that opening?



There is an informational [website](#) with example photos that discusses Glazing Bands per **AS1428.1** (an Australian standard). The minor distortion (see above) may be enough to not only increase good design features but may also have the effect of reducing potential litigation resulting from injuries (well, not certain it will help with birds, but remember, not everyone who is visually impaired is blind). Worth considering, if not already doing now.

Q + A regarding CBUs

Q1: Where multiple CBUs are located on a site, is it possible to provide mailboxes for all accessible units at one CBU location instead of designing every CBU location to be accessible?

A1: Unfortunately, it is not. CBUs are considered *common use* areas. [See **NCBC 1102** definition of COMMON USE.] An accessible route is required to all *common use* areas on a site. While **NCBC 1109.14.1** and **NCBC 1109.14.2** both have language limiting access to only 25%, or a minimum of one of each type, of *recreational facilities*, CBUs are not recreational facilities. None of the applicable documents [**NCDOT**, **USPS**] has any language limiting the number of accessible CBUs.

Q2: If the local postmaster indicates that the postal worker will deliver mail to the door of individuals with disabilities, are CBUs still required to be accessible and on an accessible route?

A2: Such an agreement is an Operational Agreement that may vary from location to location. Operational Agreements are subject to change based on changes in personnel. Such an agreement may also not address temporary disabilities that occur where postal recipients would have to *request* a change in mail delivery for an indefinite period. Building code requirements regarding access are based on an individual not having to specifically *request* accessible service. The intent is that services for both able-bodied persons and persons with disabilities be equally provided.

Q3: For a covered mail stop with multiple CBU units, what is the minimum accessible route requirement between each CBU unit? Does the minimum accessible route change if it is single-loaded (one side only) rather than double-loaded (both sides) with mail boxes?

A3: Based on the USPS document, the minimum accessible route requirement to all CBU components is now 60" clear along the entire length of the CBU component. The minimum 60" width will apply for both single- and double-loaded CBU components.

Q4: May the parking required at the CBU be shared with the parking required or provided for other areas, such as, but not limited to, clubhouses, pools, and recreational areas?

A4: No. Parking for the CBUs is dedicated solely for that purpose. The stated intent in both the USPS and NCDOT documents is to design and site CBUs for ready access. If the parking were to be shared with another use, the parking spaces required by the USPS document might not be available when a resident arrived to check and retrieve their mail.

Q5: Are there specific parking requirements, i.e., numbers of parking spaces, required at the CBUs?

A5: Yes, the **NCDOT** document, under Turnout Requirements, required the following for each CBU site:

AADT = Annual average daily traffic

1. 'A paved turnout for each CBU site.'
 - a. There is an exception that reads:

'As an exception, no turnout will be required on cul-de-sacs with curb and gutter and roadways having an AADT of 200 vehicles or less, with curb and gutter and an adjacent lane width if 15' or greater and no designated bike lane is provided.'
 - b. While NCDOT may not require a paved turnout in the above location, **NCBC 1102** still defines the area as a *common use space* and requires a minimum of either a passenger loading zone per ANSI 503 or two parking spaces, one van accessible per NCBC 1106.5 with an access aisle complying with ANSI 502.
2. 'A minimum 25' of full width vehicle storage, exclusive of transition tapers, shall be constructed to serve up to 20 postal patrons.'
 - a. 9' van space + 9' access aisle + 9' regular parking space = 27' [This is the minimum parking that the **2012 NCBC 1106.1** requires.]
3. 'An additional 25' of vehicle storage, exclusive of transition tapers, will be required for each additional 20 postal patrons, or fraction thereof.'

Q6: If parking is immediately in front of, and perpendicular to, the sidewalk in front of the CBUs, are wheel stops required at each parking space to maintain the clear width along the sidewalk?

A6: It depends on whether there is sufficient sidewalk area to maintain the more restrictive dimension of the following clear path requirements when vehicle(s) are parked:



- a. Minimum 5'-4" (64") from face of CBU to outside edge of sidewalk (**USPS**);
- b. Minimum 5'-6" (66") from the face of CBU to outside edge of sidewalk (**NCDOT + NCBC**). Item b represents the **ANSI 403.5.2** exterior path requirement of 60" to permit passing spaces plus the 2'-6" vehicle overhang that is recognized by NCDOT. If less than a 2'-6" vehicle overhang is provided (without installing wheel stops), then a parked car will reduce the **NCBC 1104.2** accessible route to less than the 48" exterior accessible route requirement.

NOTE: if a curb is provided, the width of the curb is not included in the accessible route measurements.



STATE OF NORTH CAROLINA
DEPARTMENT OF TRANSPORTATION

PAT MCCRORY
GOVERNOR

NICHOLAS J. TENNYSON
SECRETARY

September 1, 2015

MEMORANDUM TO: Division Engineers
District Engineers

FROM: Mike Holder, P.E. 
Chief Engineers

SUBJECT: Policy for Placement of Mail Cluster Box Units in Subdivisions

Several months ago the United States Postal Service (USPS) began implementing a policy for centralized delivery of mail for new developments. This equates to requiring developers of residential subdivisions to provide necessary mail receptacle equipment for centralized mail delivery, i.e. cluster box units (CBU's) in new developments. Most transportation agencies and developers did not know about USPS's new requirement until far into the planning phase, or build out phase of new subdivisions.

As a response to the many requests from land developers and builders, a work group was assembled by the Chief Engineer's Office to study the impacts of accommodating the CBUs on residential subdivision streets. The work group met with industry partners on several occasions and developed a policy for accommodating CBUs on subdivision streets. While the safest and most preferred placement of CBUs is outside the right-of-way, there are cases where it is not feasible to do so.

The attached document will serve as the Department's policy when reviewing plans for placement of CBUs on right-of-way for subdivision streets. This document will also serve for plan review where developers must retrofit subdivisions already built with CBUs and encroach on right-of-way. I think you will find the policy firm enough to determine acceptable safe conditions for typical placement of the CBU's, and flexible enough to make wise decisions for new cases as they evolve.

Please distribute this to the appropriate staff in your Division. If you have questions or need clarification on this policy, please contact Delbert Roddenberry, Operations Program Manager at 919.707.2509.

MLH/drr

Attachment

CC: Delbert Roddenberry,
file

MAILING ADDRESS:
NC DEPARTMENT OF TRANSPORTATION
OFFICE OF THE CHIEF ENGINEER
1536 MAIL SERVICE CENTER
RALEIGH NC 27699-1536

TELEPHONE: 919-707-2500
FAX: 919-733-9428

WEBSITE: WWW.NCDOT.GOV

LOCATION:
TRANSPORTATION BUILDING
1 SOUTH WILMINGTON STREET
RALEIGH NC

Placement of Mail Cluster Box Units (CBU) on State Maintained Subdivision Streets
Policy September 1, 2015

The purpose of this policy is to provide administrative and design guidelines for safe and efficient accommodation of delivery and receipt of mail in residential subdivisions or business developments by use of cluster box units (CBU) placed centrally or distributed in groups. These guidelines will apply to roadways eligible for addition to the State system with design speeds of 35 MPH or less within the subdivision/community as referenced in the North Carolina Department of Transportation Subdivision Roads Minimum Construction Standards January -2010.

This policy is written with consideration to United States Postal Service (USPS) guidelines, AASHTO Policy on Geometric Design of Highways and Streets, AASHTO Roadside Design Guide, 4th Edition, the NC Administrative Code, and the NC General Statutes. Access to CBU's must be provided in compliance with Americans with Disabilities Act (ADA) accessibility guidance.

CBU's are specialized multiple mailbox installations containing a cluster of 8 to 16 or more locked boxes serving multiple postal patrons and mounted on a pedestal or framework per USPS requirements. Multiple CBU's may be located at a mail stop or kiosk at one or more locations within a development as allowed by USPS. Generally, CBU's have such mass that they are not considered crashworthy and must be located outside of the clear area needed for safe recovery of errant vehicles.

Preference shall be given to placement of CBU's in subdivision or development common areas and outside of the road right of way to provide safe and convenient parking and pedestrian access for postal patrons and to minimize impedance to roadway operation. Under extenuating circumstances when such common areas are justifiably not available due to the nature of the development, CBU's may be accommodated within the road right of way in accordance with this policy and subject to approval of the District Engineer

Location Requirements:

CBU sites shall be located internal to the development and shall be located on the right hand side of the roadway in the direction of travel. In general, CBU's should be located in common areas, near lot lines, outside of public utility easements, private storm drainage easements, well outside any clear zones/clear recovery areas and at least 10 feet away from water meters, cleanouts, and hydrants. CBU sites must be selected to preserve necessary lines of sight at intersections, driveways, and horizontal and vertical curves. CBU's shall be located outside the influence area of adjacent intersections and not impede driveways or other functional areas of access. The units shall be 100' minimum from minor street intersections, 200' minimum from primary route intersections. CBU sites shall be located so as to minimize unnecessary travel distance, backing or U-turns. CBU sites shall not be located directly across from one another on opposing sides of the road. Under no circumstances shall the units be placed such that traffic will be negatively affected on existing or proposed streets outside of the subdivision or development.

Turnout Requirements:

A paved turnout shall be required for each CBU site. The turnout pavement shall be consistent with the adjacent roadway pavement structure. A minimum of 25' of full width vehicle storage, exclusive of transition tapers, shall be constructed to serve up to 20 postal patrons. An additional 25' of vehicle

storage, exclusive of transition tapers, will be required for each additional 20 postal patrons, or fraction thereof, that are served at any CBU site. This treatment is applicable to both streets with unpaved shoulder sections, and curb and gutter sections with and without sidewalk. ADA accessibility and compliance must be maintained in each instance. As an exception, no turn out will be required on cul-de-sacs with curb and gutter and roadways having an AADT of 200 vehicles or less, with curb and gutter and an adjacent lane width of 15' or greater and no designated bike lane is provided.

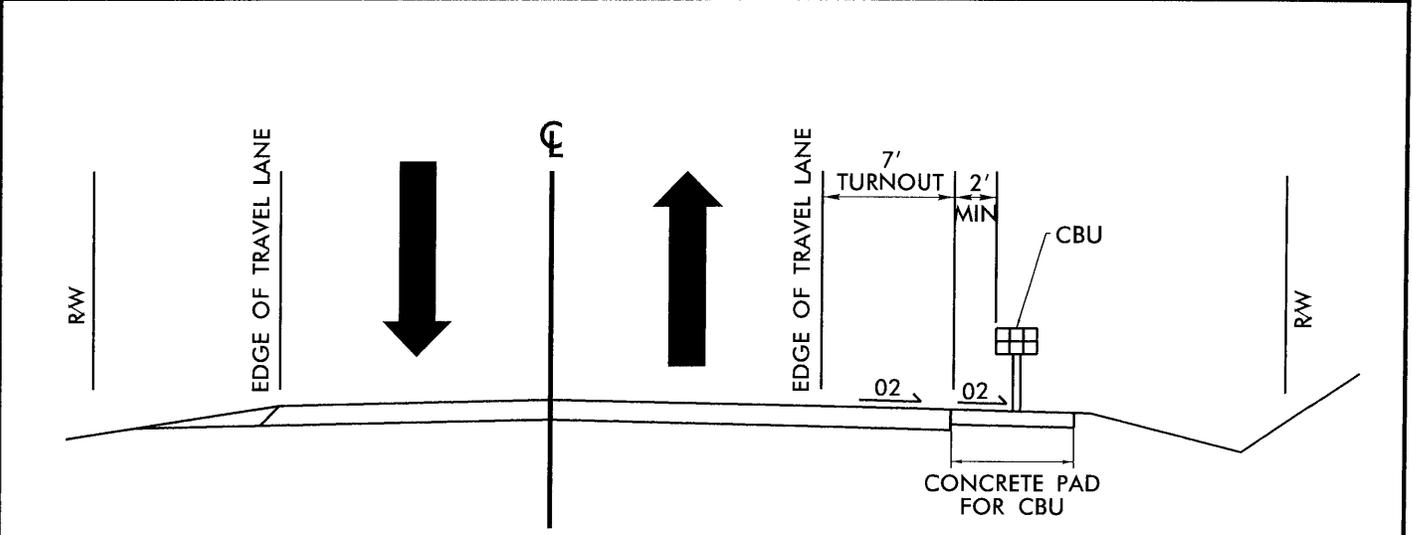
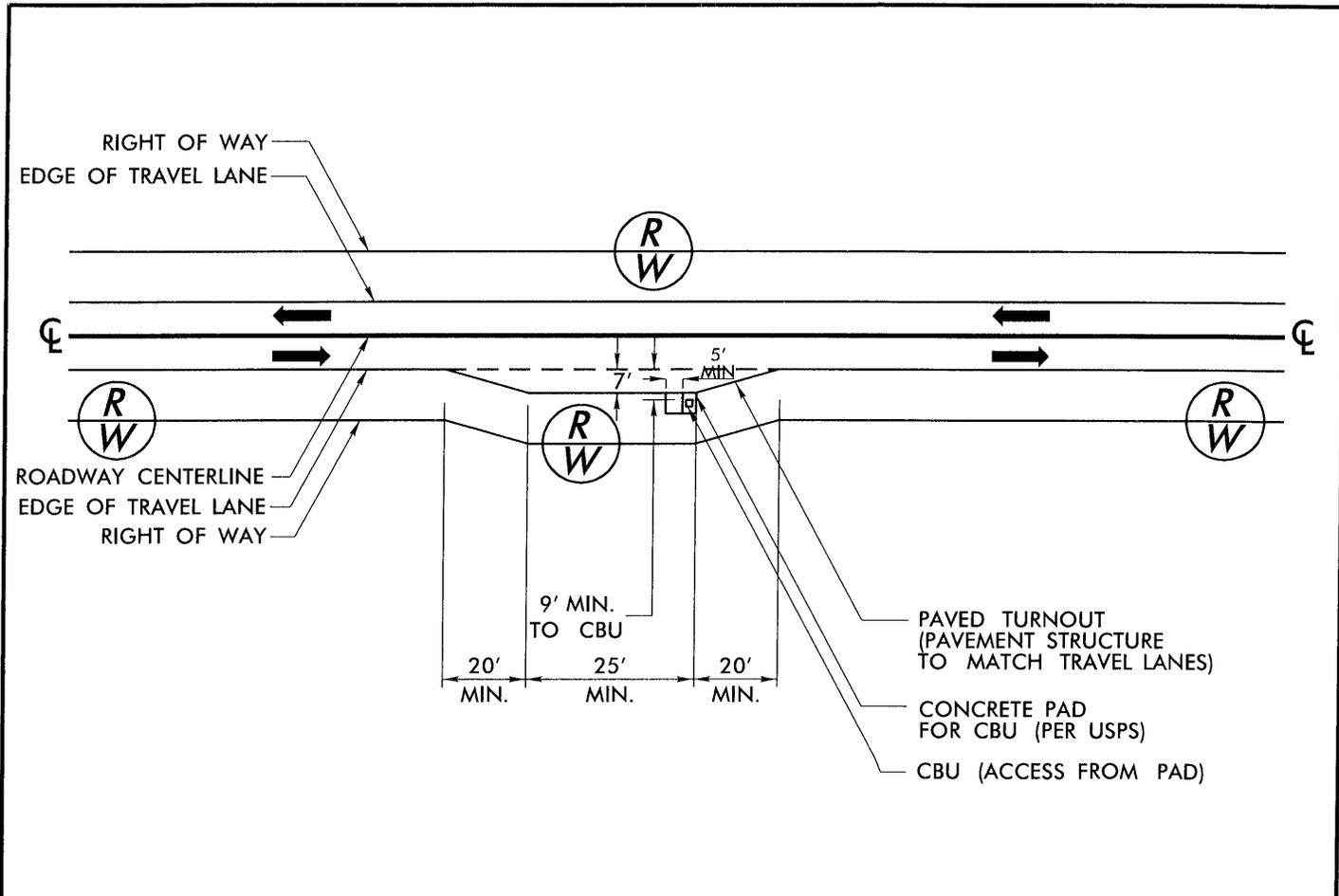
Minimum Design Requirements:

Minimum required design criteria for CBU sites located within the right of way are shown in the attached figures 1 thru 4. Prior to installation, the District Engineer shall review and approve the proposed site placement and design for each CBU. Subdivision plans submitted to the Department shall clearly indicate all proposed CBU site locations and provide appropriate design detail. The designer shall independently coordinate with the USPS and local government for review and approval. Approval of the CBU installation by the Department, in no way constitutes acceptance by the USPS or assurance of mail delivery.

Road Addition and Maintenance Responsibilities:

Prior to road addition, the District Engineer shall inspect each CBU site. If safety, operational or maintenance issues become evident during the road addition process, the CBU will be relocated, modified, or repaired at no cost to the Department prior to road addition.

Upon addition of the road to the State system, NCDOT shall assume maintenance of the paved turn out, shoulder and curb and gutter, ditch, and storm drainage within the right of way in accordance with established minimum maintenance standards. NCDOT shall not own or maintain CBU's or other appurtenances associated with these units such as concrete pads, sidewalk, and wheel chair ramps. The CBU and appurtenances shall be considered an encroachment and administered in accordance with the current edition of the [Policies and Procedures for Accommodating Utilities on Highway Rights of Way](#). In addition, NCDOT is not responsible for clearing snow, ice, limbs, trash accumulation, specialized vegetation management, etc. from associated sidewalk, parking areas (if applicable), and the immediate area around the CBU. These maintenance activities must be provided by mailbox users or a homeowner's association as a condition of the encroachment agreement.



SEE FIGURE 1 PAGE 39, "NCDOT SUBDIVISION ROADS MINIMUM CONSTRUCTION STANDARDS JANUARY 2010" FOR LOCAL AND COLLECTOR ROAD DIMENSIONS.

NOTE: MAINTAIN R/W OFFSET AROUND CBU TURNOUT

FIGURE 1
CBU PLACEMENT FOR SHOULDER SECTION RESIDENTIAL LOCAL AND COLLECTOR SUBDIVISION STREETS

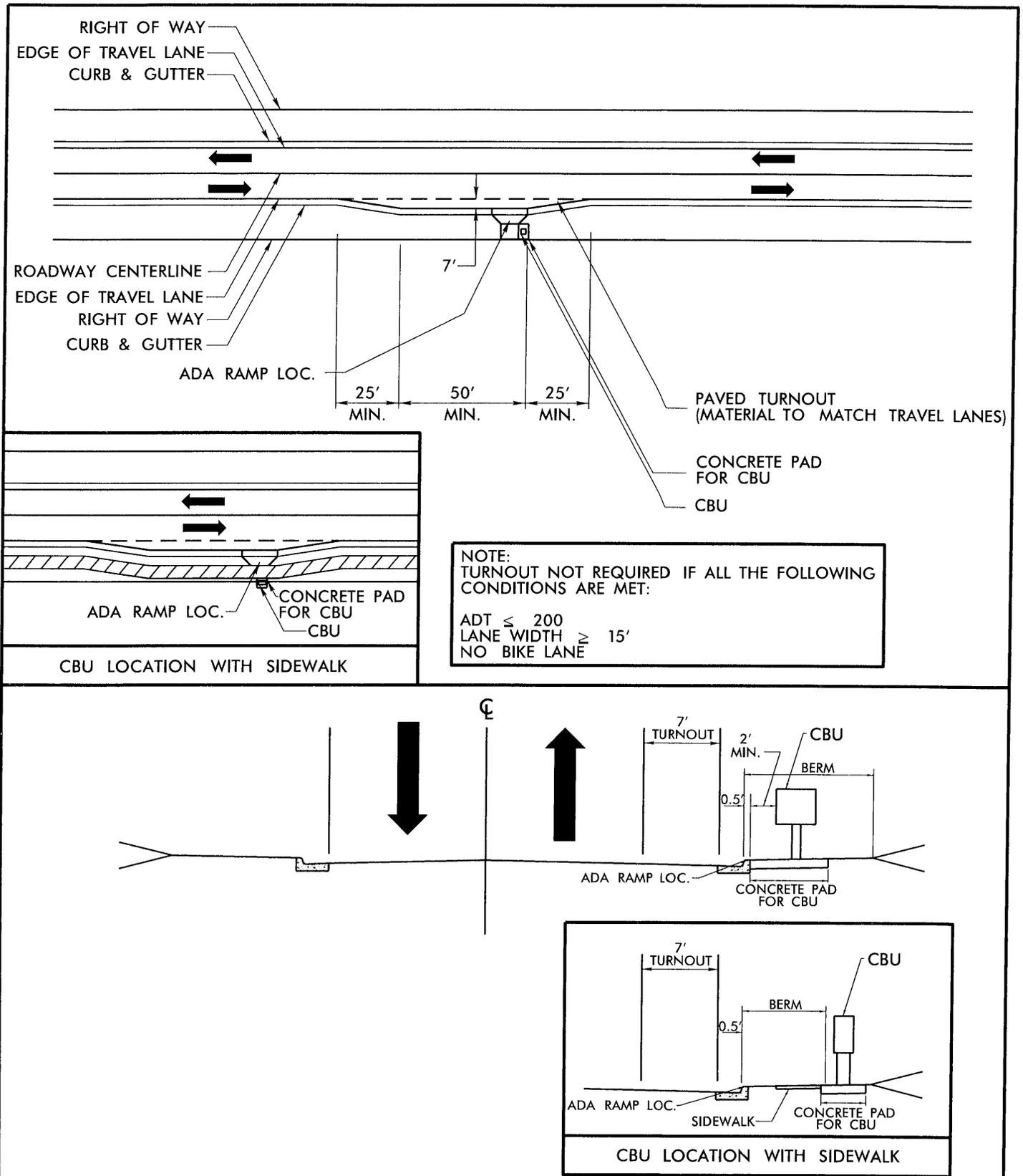
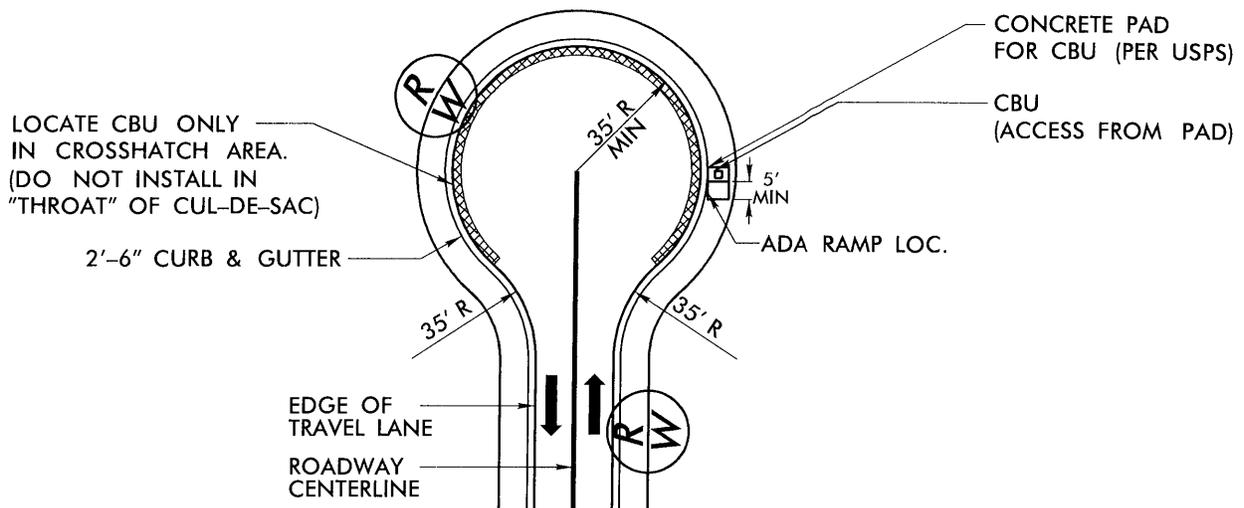


FIGURE 2

CBU PLACEMENT FOR C & G SECTION RESIDENTIAL LOCAL AND COLLECTOR SUBDIVISION STREETS



SEE FIGURE 7 PAGE 45, "NCDOT SUBDIVISION ROADS MINIMUM CONSTRUCTION STANDARDS JANUARY 2010" FOR LOCAL AND COLLECTOR ROAD DIMENSIONS.

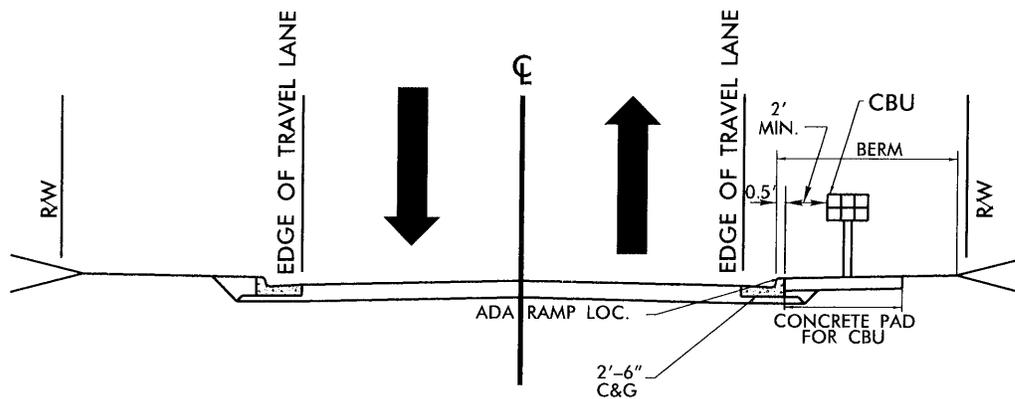
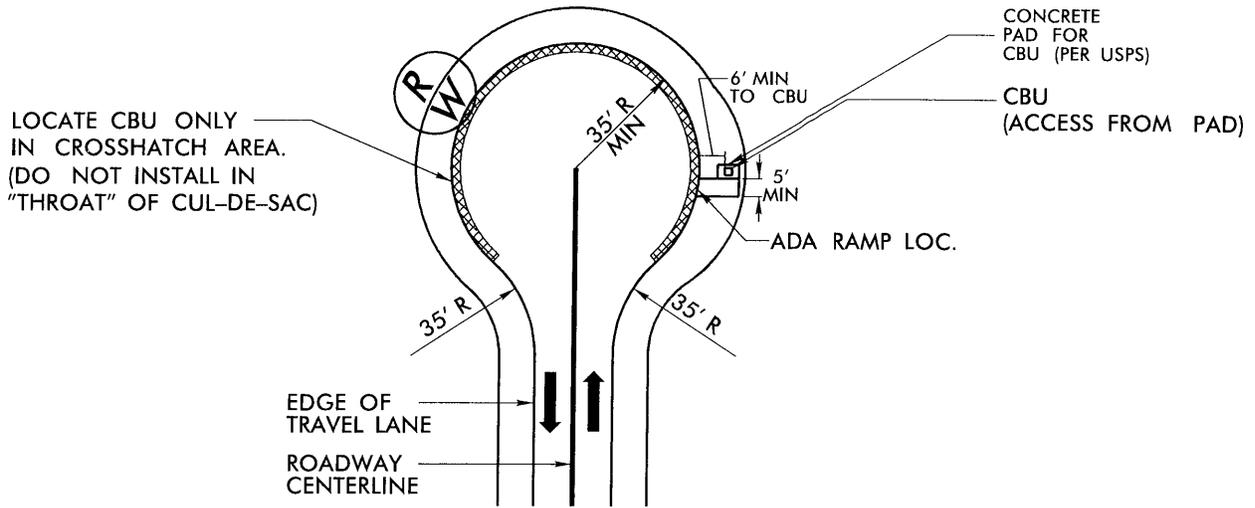


FIGURE 3

**CBU PLACEMENT FOR C & G SECTION RESIDENTIAL SUBDIVISION STREETS
CUL-DE-SAC**



SEE FIGURE 8 PAGE 46, "NCDOT SUBDIVISION ROADS MINIMUM CONSTRUCTION STANDARDS JANUARY 2010" FOR LOCAL AND COLLECTOR ROAD DIMENSIONS.

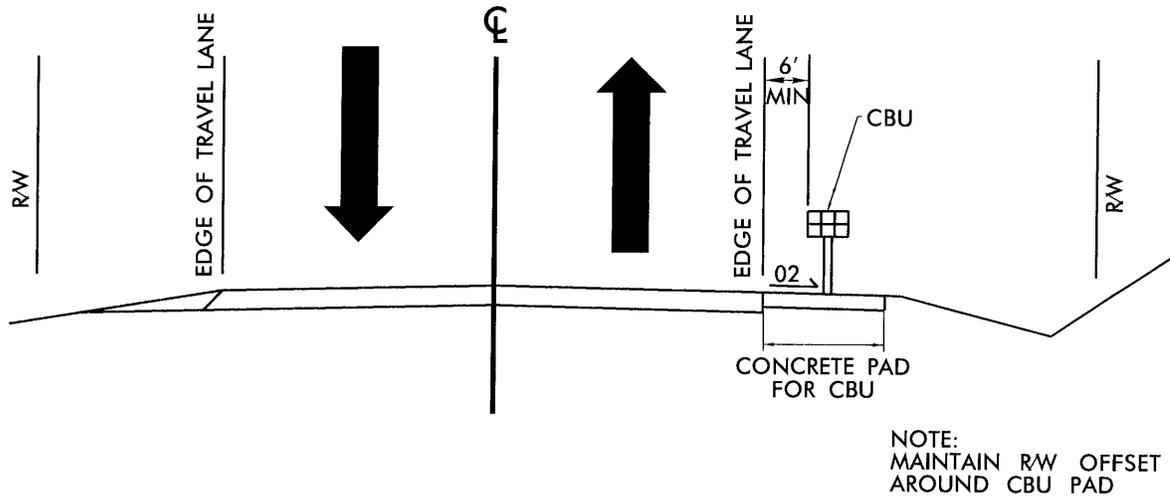


FIGURE 4

CBU PLACEMENT FOR SHOULDER SECTION RESIDENTIAL SUBDIVISION STREETS
CUL-DE-SAC

February 9, 2017

AREA VICE PRESIDENTS
DISTRICT MANAGERS

SUBJECT: Cluster Box Units (CBU) Concrete Pad Installations

Recently, questions have arisen related to the installation of concrete pads for Cluster Box Units (CBU) and the applicability of handicap accessibility requirements to those pads. In the interest of offering accessible service to all of our customers, please find attached the interim policy for CBU Concrete Pad Installations.

Effective immediately, the guidelines in the CBU Concrete Pad Installations must be followed for all installations and repairs. Further, USPS will only proceed with CBU concrete pad installations for conversions or locations where USPS maintenance responsibility has been documented in the change in delivery agreement. Any deviation to the attached policy must be submitted by the Area Vice President to the Vice President, Delivery Operations for review and approval prior to proceeding.

If you have any questions, please contact Jacqueline Cooks at 202-268-5848 or Craig Ballard at 303-264-0461.



Tom A. Samra
VP, Facilities



Kevin L. McAdams
VP, Delivery Operations

**Facilities Repair & Alterations
Interim Policy for
CBU Concrete Pad Installations**

January 27, 2017

In accordance with USPS Policy, the District Growth Coordinator or designee verifies the following:

- In compliance with USPS policy, Facilities will not install CBU pads for new deliveries. USPS can dictate the mode of delivery. New CBUs and pads should be installed by the developer.
- USPS has installed many CBUs and pads over the years, but that does not automatically mean that USPS is responsible for maintenance or replacement. Facilities will only repair and/or replace existing CBU pads when there is a written contract showing that USPS is responsible.

District Growth Coordinator or designee verifies and assembles required documents:

- Number of Cluster Box Units (CBU) to be installed.
- Photographs of the proposed installation location and surrounding area. If a replacement of existing damaged CBU concrete slabs, photographs to include affected pads and surrounding area.
- Executed agreement form with the property owner / landlord allowing the U.S. Postal Service to install concrete pads on their property. Agreement to identify property owner / landlord as having full maintenance of the concrete pads including snow removal.
- Approved funding eBuy on the District finance number for the full cost of the CBU concrete pad installation project. Assume \$1,000.00 per CBU unit to be installed.
- All of the above documents constitutes the “Scope of Work Package” and needs to accompany the CBU Concrete Pad Request form.

District Growth Coordinator or designee submits completed Scope of Work package to Facilities Response Line Self Service (on Blue Page) to be entered and assigned to FMO/MO for Article 32 consideration.

- If FMO/MO accepts the work in FSSP then they will contact the Growth Coordinator or designee for transmittal of required scope of work documents to them.
- If FMO/MO accepts the work and completes the concrete pad installation, then Growth Coordinator or designee completes the Simplified Certificate of Accessibility and includes photographs of completed installation from FMO/MO.
- If FMO/MO declines the work in the FSSP system the work will be assigned to a Facilities designated Project Manager for completion.

If the CBU Concrete Pad request is declined by FMO/MO, it is then assigned to the Facilities designated Project Manager for that Area:

- Facilities Project Manager contacts the Growth Coordinator or designee for transmittal of required scope of work documents.
- Once documents are received, the Facilities Project Manager contacts recommended contractor and/or other known contractor to acquire proposals. Design to be in accordance with USPS standard details including wheelchair turning radius (attached) and adequate concrete sidewalk to provide an accessible route back to the nearest existing sidewalk, parking area, public access point, or foot path. No modification to existing parking spaces or any other features is required.
- The Facilities Project Manager reviews and concurs with the contractors design documents.
- Project Manager to award work to appropriate local contractor. No electrical, lighting, or physical shelters are to be included in the scope of work.
- Contractor completes work.
- FMO/MO, at the time of the CBU equipment installation, verifies the work is completed per the scope of work given to contractor and the approved design documents. Then the FMO/MO photographs of the completed installation.
- Growth Coordinator or designee completes the Simplified Certificate of Accessibility and includes photographs of completed installation from FMO/MO and provides a completed copy of the Simplified Certificate of Accessibility to the Facilities Project Manager.
- The Facilities Project Manager will have the photos and CoA signed by the concrete contractor. He should send an electronic copy of these documents to the District Growth Coordinator Rep who will sign the CoA. The Growth Coordinator Rep will then add the photos of the CBUs submitted by FMO/MO and sign the CoA. An electronic copy of the complete CoA document will then be submitted to the Facilities Project Manager so they can close out the project.
- Facilities Project Manager pays contractor upon receipt of a complete invoice package which demonstrates the concrete slab was installed properly. A Simplified Certificate of Accessibility signed by the Growth Coordinator or designee is not required for payment to the contractor.
- Growth Coordinator or designee keeps the original Simplified Certificate of Accessibility and other pertinent documents on file for future coordination.

REQUEST FOR CBU CONCRETE PAD INSTALLATION
Delivery Growth Management

Number of CBU Units: _____ Date: _____
Type of CBU Units: _____ Control No.: _____
Type of Installation: New Development: Conversion: Replacement:

WHERE WILL THE CBU(S) BE INSTALLED?

Street Address: _____
City: _____ State: _____
ZIP + 4: _____ - _____

Subdivision (if known): _____

Section (if known): _____

Nearest Cross Street: _____

Verbal Description: _____

On-Site POC Name: _____

On-Site POC Phone: _____

USPS POC Name: _____

USPS POC Phone: _____

Contractor Name (if known): _____

Contractor Phone (if known): _____

CHECKLIST OF REQUIRED ATTACHMENTS

- Photographs of the proposed installation location and surrounding area
- Executed agreement form with property owner or landlord
- Approved funding eBuy on the District finance number

Requestor Name: _____

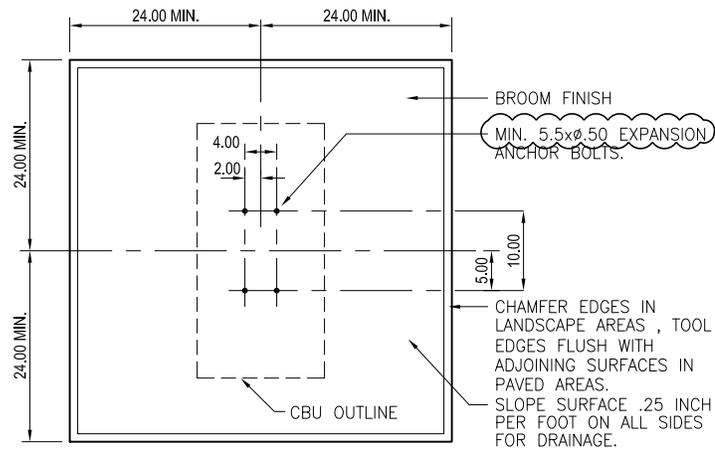
Requestor Title: _____ Phone: _____

Growth Coord. Name: _____

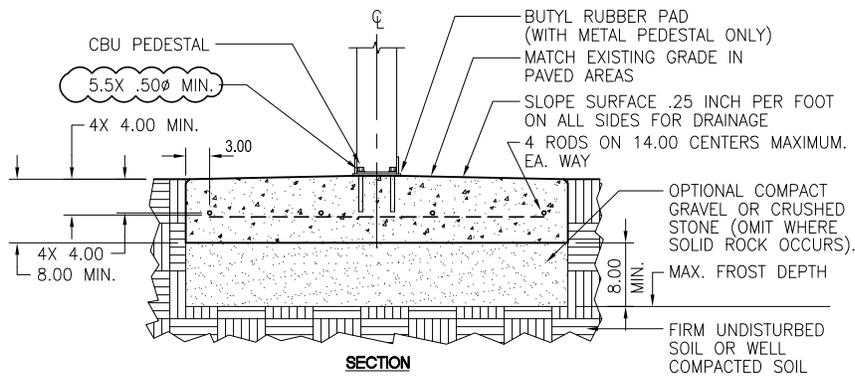
District: _____ Phone: _____

Growth Coordinator Signature: _____

NOTES TO A/E:



PLAN



SECTION

NOTES:

1. CONCRETE SHALL HAVE A COMPRESSIVE STRENGTH OF 3,000 PSI @ 28 DAYS, CONTAIN 4% MIN. - 6% MAX. AIR ENTRAINMENT AND BE PLACED WITH A 3.50-4.50 SLUMP IN ACCORDANCE WITH ACI 301
2. REINFORCING STEEL RODS SHALL CONFORM TO ASTM A615, GRADE 60.

3. EXPANSION BOLTS SHALL BE EQUIVALENT TO THE FOLLOWING PROVIDERS:
 - a. HILTI KWIK BOLT (www.us.hilti.com) II-1/2" DIAMETER x 5-1/2" OVERALL LENGTH GALVANIZED, CATALOG #: 000-453-696
 - KB II 12-512, STAINLESS STEEL; CATALOG #: 000-454-744
 - ENSURE THAT THE MIN. EMBEDMENT IN CONCRETE IS AT LEAST 3-1/2".

- b. ITW RAMSET REDHEAD TRUBOLT (www.ramset-redhead.com) GALVANIZED, 1/2" DIAMETER x 7" OVERALL LENGTH; CATALOG NUMBER: WS-1270G
- ENSURE THAT THE MIN. EMBEDMENT IN CONCRETE IS AT LEAST 4-1/8".

- c. RAWL STUD (www.rawl.com) GALVANIZED, 1/2" DIAMETER x 5-1/2" OVERALL LENGTH; CATALOG NUMBER: 7724
- ENSURE THAT THE MIN. EMBEDMENT IN CONCRETE IS AT LEAST 4".

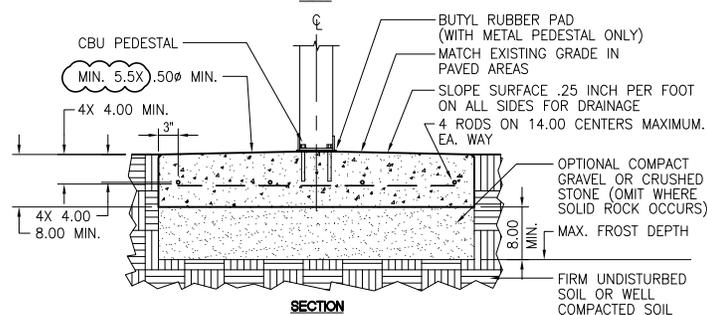
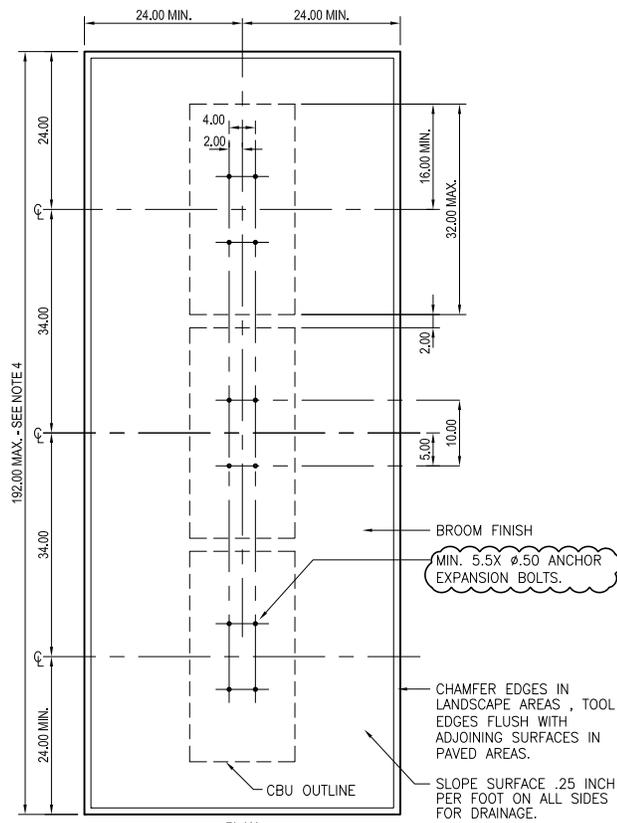
Detail: CLUSTER BOX UNIT (CBU) INSTALLATION - SINGLE UNIT

Fac.	Ch.	Sect.	Para.	Detail
				G1-2-0e

CAD File: [../usps/library/details/G1-2-0e](http://usps/library/details/G1-2-0e) Scale: 1/2" = 1'-0"

USPS SDL Issued: 10/1/2016
Last Revised: 10/27/2016

NOTES TO A/E:



NOTES:

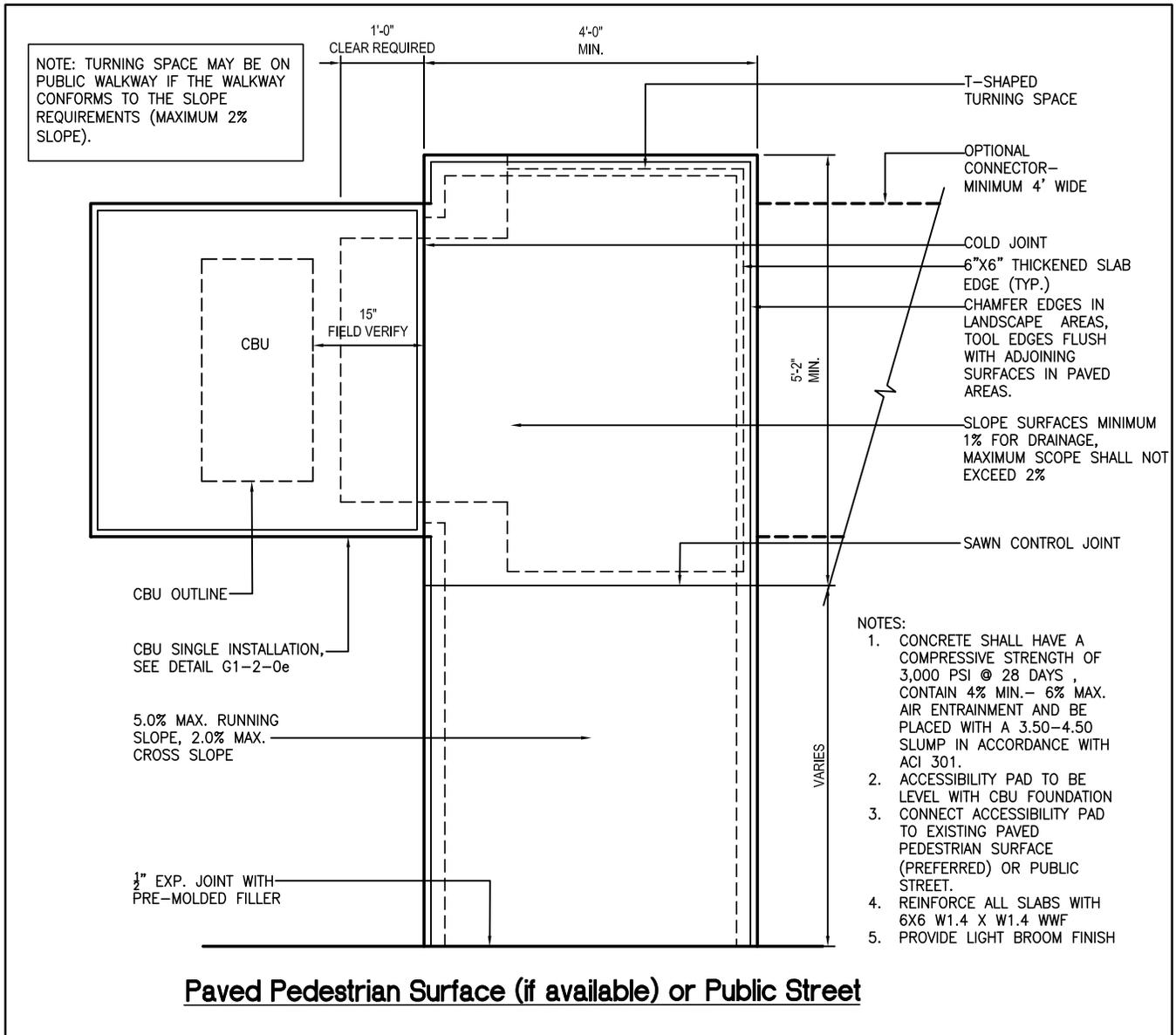
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2. REINFORCING STEEL RODS SHALL CONFORM TO ASTM A615, GRADE 60.
3. EXPANSION BOLTS SHALL BE EQUIVALENT TO THE FOLLOWING PROVIDERS:
 - a. HILTI KWIK BOLT (www.us.hilti.com) II-1/2" DIAMETER x 5-1/2" OVERALL LENGTH
GALVANIZED, CATALOG #: 000-453-696
KB II 12-512, STAINLESS STEEL; CATALOG #: 000-454-744
ENSURE THAT THE MIN. EMBEDMENT IN CONCRETE IS AT LEAST 3-1/2".
 - b. ITW RAMSET REDHEAD TRUBOLT (www.ramset-redhead.com)
GALVANIZED, 1/2" DIAMETER x 7" OVERALL LENGTH; CATALOG NUMBER: WS-1270G
ENSURE THAT THE MIN. EMBEDMENT IN CONCRETE IS AT LEAST 4-1/8".
 - c. RAWL STUD (www.rawl.com)
GALVANIZED, 1/2" DIAMETER x 5-1/2" OVERALL LENGTH; CATALOG NUMBER: 7724
ENSURE THAT THE MIN. EMBEDMENT IN CONCRETE IS AT LEAST 4".
4. A 3 CBU CONFIGURATION IS DEPICTED. A 2 OR 4 CBU CONFIGURATION MAY BE USED AS LONG AS THEY ARE ARRANGED IN GROUPS SUCH THAT THE OVERALL DIMENSION OF THE CONCRETE BASE DOES NOT EXCEED 16 FEET.

Detail:	CLUSTER BOX UNIT (CBU) INSTALLATION - MULTIPLE UNIT	Fac.	Ch.	Sect.	Para.	Detail
						G1-2-0 e1
CAD File:	../usps/library/details/G1-2-0e1	Scale:	1/2" = 1'-0"			
				USPS SDL Issued:	10/1/2016	
				Last Revised:	10/27/2016	



NOTES TO A/E:

- IF THE ACCESSIBLE ROUTE FROM THE CBU(S) CONNECTS WITH A STREET OR OTHER PAVED SURFACE AT A VERTICAL CURB, A CURB RAMP SHOULD BE INSTALLED IN ACCORDANCE WITH RE-4 REQUIREMENTS.

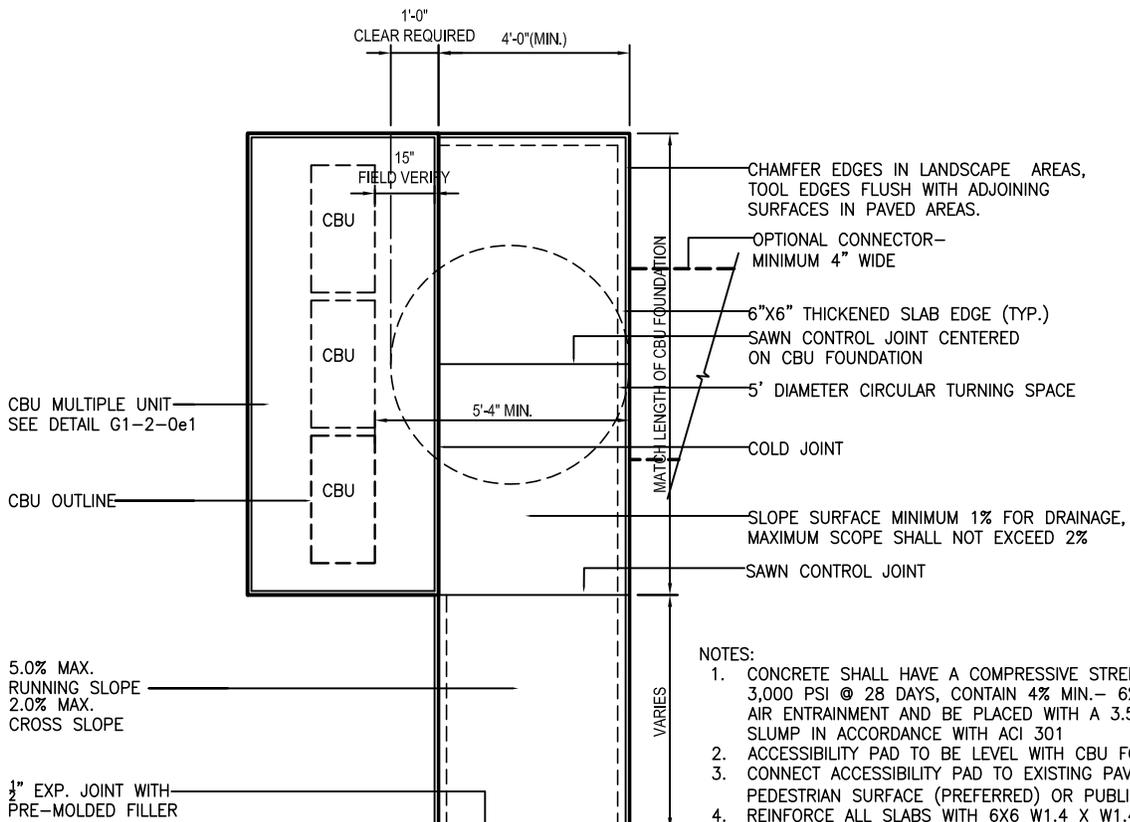


Detail:	CLUSTER BOX UNIT (CBU) ACCESS MANEUVERING SPACE - SINGLE UNIT				Fac.	Ch.	Sec.	Para.	Detail
					G1 - 2 - 0 (e2)				
CAD File:	..\\usps\\library\\details\\G1-2-0e2.dwg			Scale:	1/2" = 1'-0"		0 1/2" 1"		
				USPS SDL Issued:	10/1/2016		Last Revised: 7/14/2016		

NOTES TO A/E:

- IF THE ACCESSIBLE ROUTE FROM THE CBU(S) CONNECTS WITH A STREET OR OTHER PAVED SURFACE AT A VERTICAL CURB, A CURB RAMP SHOULD BE INSTALLED IN ACCORDANCE WITH RE-4 REQUIREMENTS.

NOTE: TURNING SPACE MAY BE ON PUBLIC WALKWAY IF THE WALKWAY CONFORMS TO THE SLOPE REQUIREMENTS (MAXIMUM 2% SLOPE).



- NOTES:
- CONCRETE SHALL HAVE A COMPRESSIVE STRENGTH OF 3,000 PSI @ 28 DAYS, CONTAIN 4% MIN.- 6% MAX. AIR ENTRAINMENT AND BE PLACED WITH A 3.50-4.50 SLUMP IN ACCORDANCE WITH ACI 301
 - ACCESSIBILITY PAD TO BE LEVEL WITH CBU FOUNDATION
 - CONNECT ACCESSIBILITY PAD TO EXISTING PAVED PEDESTRIAN SURFACE (PREFERRED) OR PUBLIC STREET.
 - REINFORCE ALL SLABS WITH 6X6 W1.4 X W1.4 WWF
 - PROVIDE LIGHT BROOM FINISH

Paved Pedestrian Surface (if available) or Public Street

Detail	CLUSTER BOX UNIT (CBU) ACCESS MANEUVERING SPACE - MULTIPLE UNIT	Fac.	Ch.	Sect.	Para.	Detail
CAD File: ..\\usps\\library\\details\\G1-2-0e3.dwg		G1 - 2 - 0 e3				
Scale:	1/4"=1'-0"					